

WINNIPEG RIVER BRIDGE ON PR 313



Previous newsletters have provided information on the history of the bridge, purpose and need for the project, stakeholder input and feedback, bridge design option evaluation, and construction time-lines. These past newsletters can be found on the project website (www.gov.mb.ca/mit/wcs/constructionproj.html).

New Bridge Design

MI has completed an extensive engineering analysis for the bridge repair and has considered several bridge rehabilitation and replacement options. After extensive community consultation, and discussion with local municipalities, the decision was made to construct a new bridge on the current alignment utilizing the existing piers. The decision was based on a detailed analysis of the design options, and provided significant consideration for the affect that a bridge closure would have on the travelling public and other stakeholders. The selected design will allow the new bridge to be constructed in stages, without closing the bridge for the majority of the construction period. (See '**Construction**' for more information.) This design will provide a new wider facility that will improve service to the region for the next 40 years.

The existing bridge is 6.2m (20 feet) wide, while the new bridge (Figure 1) will be 9.6m (31.5 feet). The new bridge will include a new sidewalk on the north side (same location as the current sidewalk).

Local municipalities had previously expressed a desire to modify the bridge design in order to increase the navigational clearance of the bridge to 4.6m (15 feet) from the original design of 3.8m (12.5 feet). As this change required additional time and budget, the local communities were advised that they would need to contribute a portion of the incremental costs associated with raising the bridge in order to proceed. The local community responded that they were not able to provide the additional funding and the decision was made to proceed with the original design clearance of 3.8m (12.5 feet).

The existing condition of the bridge has continued to deteriorate and minor temporary repairs will be required in order to proceed with the remainder of construction. A structural analysis and temporary repair is being evaluated.

The delays and additional work have postponed the anticipated completion of the project to 2019. On-site work activities with respect to the bridge re-construction will not commence until the spring of 2018. Manitoba Infrastructure remains committed to the project; the first phase of substructure modifications has been completed (Fall 2016), structural steel (girder) fabrication is currently under way, and additional contracts for the temporary repair and precast concrete deck panel fabrication will be issued for tender in Fall 2017.

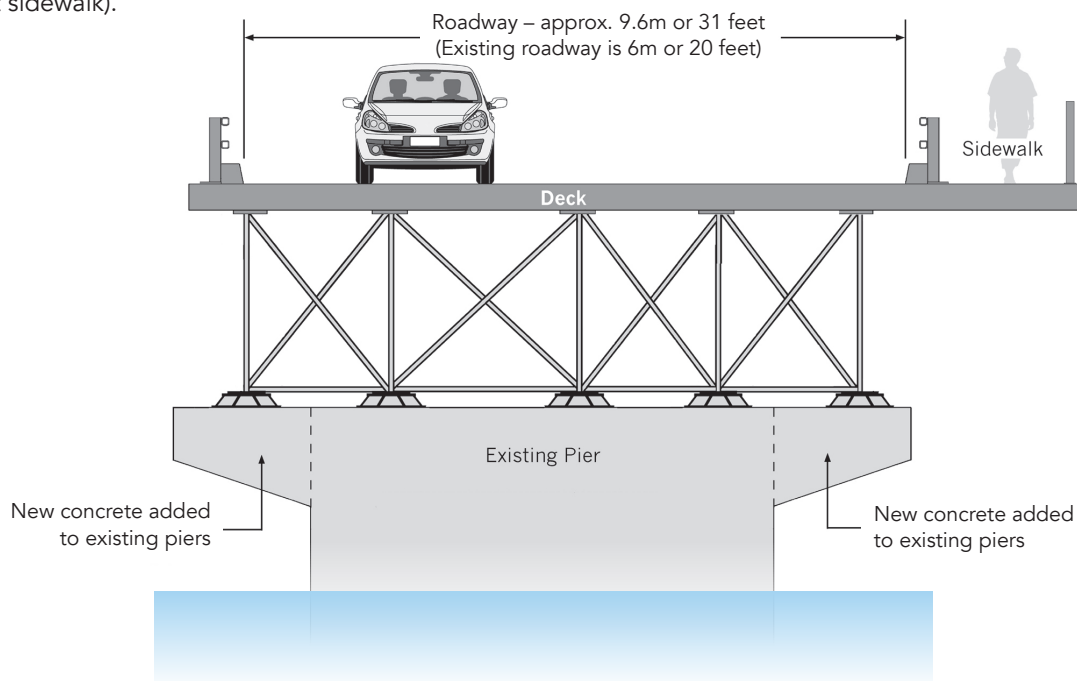


Figure 1: Build New Bridge on Existing Piers

Construction

The bridge will continue to operate in its current condition (one lane controlled with signals) through the entire construction period, with the exception of a short closure period. It will be necessary to close the bridge completely to traffic for approximately three weeks at some point during the winter (ie. November–April). The local community and stakeholder will be notified well in advance of this closure.

Current Status

- MI is working with Manitoba Hydro, MTS, and others to ensure that minimal disruption of service will take place during construction.
- Minor temporary bridge repairs will be conducted in Fall 2017.
- The bridge re-construction tender will be issued prior to December 2017.
- Structural steel/girder fabrication is underway and will be completed by December 2017.
- Concrete deck panel production will commence in fall 2017.
- On-site project activities will occur in Spring 2018.
- A three-week full bridge closure will take place in Winter 2018/2019 in order to replace the navigation bridge span.
- The bridge will be fully open in Fall 2019.

More Information

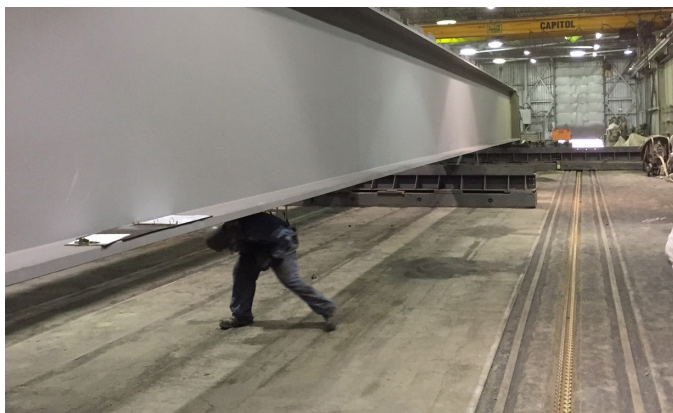
MI is aware that during peak traffic flows on long weekends, wait times at the bridge signals can be extensive. Therefore, MI will be providing additional traffic control on the July, August and September long weekends, in order to mitigate delays. Please know that the signals are operating as efficiently as possible, in absence of flag people, and we thank you for your continued patience.

Additional information, including a full consultation report and an updated project status report can be found on the project website at:

www.landmarkplanning.ca/current-projects.php

or

www.gov.mb.ca/mit/wcs/constructionproj.html



Structural steel girder



Engine # 1 – 1913

PR 313 Bridge History

The study team will be working with the Lac du Bonnet and District Historical Society Inc. to incorporate the history of the bridge into the design of the new bridge. Discussions with the Historical Society have resulted in several concepts including naming of the bridge, a historical plaque, and the preservation of the signage currently located on the bridge. The Historical Society put out a request for feedback in the January 14, 2016 edition of the Lac du Bonnet Clipper asking for locals to provide their thoughts on naming of the bridge. Email your suggestions to annyhall50@hotmail.com.

Did you know...

The PR 313 Bridge has a rich history dating back to 1908 when the first bridge accommodated a Winnipeg Hydro Tramway. In 1931 the wooden bridge was replaced with a steel Dominion Bridge structure that was used for both rail and highway traffic. The bridge deck was later raised by four feet in order to accommodate rising water levels created by the McArthur Falls Generating Station. Further modifications and repairs were undertaken over the years including the removal of the rail tracks in 1963.

On July 15, 1870 Manitoba was created by proclamation of the Manitoba Act. The size and shape of province afforded it the nickname "The Postage Stamp Province". Interestingly, the longitude and latitude of the eastern boundary for the Province of Manitoba began at a point 96 degrees west of Greenwich and that point is located on the existing PR 313 Bridge!