

Bridge Over Red River Floodway on PTH 59

Previous newsletters provided an overview of the project, the purpose and need for the project, frequently asked questions, and the preferred preliminary design alternative. More information on this project can be found on the Manitoba Infrastructure website [here](#).



FIGURE 1 - EXISTING BRIDGE ON PTH 59

BACKGROUND INFORMATION

The existing structure on PTH 59 North over the Red River Floodway was completed in 1964. The structure has reached the end of its service life and needs replacement.

A preliminary design for the replacement of the bridge structure was completed in October 2019 (see Figure 2). The preferred alternative was chosen after public consultation with local municipalities, landowners and stakeholders from the area. Of the alternatives considered, this alternative has the lowest overall cost, requires the least amount of land acquisition, improves access to PTH 59 and

will have minor traffic disruptions during construction periods (see Construction for more information). The detailed design of the new bridges was completed in September 2020 and a tender for the construction of the new structures was issued in October 2020.

NEW BRIDGE STRUCTURES

The replacement structures will be two separated seven span bridges over 300m (or 1000 feet) long (see Figure 3). Each bridge will support three lanes of traffic (two travel lanes, and one acceleration/deceleration lane). A new two-way access loop will be constructed under the bridge with an improved off-ramp onto Oasis Road. A portion of the curve of PTH 59 east of the bridge will be also be re-aligned to provide a smoother roadway alignment.

CONSTRUCTION PHASING

The construction period will occur in two main stages and will span approximately three years (subject to contractors' final scheduling).

During Stage 1, the existing bridge will continue to operate in its current condition (two lanes southbound and one lane northbound) while a new structure is built parallel to the existing bridge. Southbound travel will experience intermittent periods of single lane travel during this phase. The access ramp under the bridge and

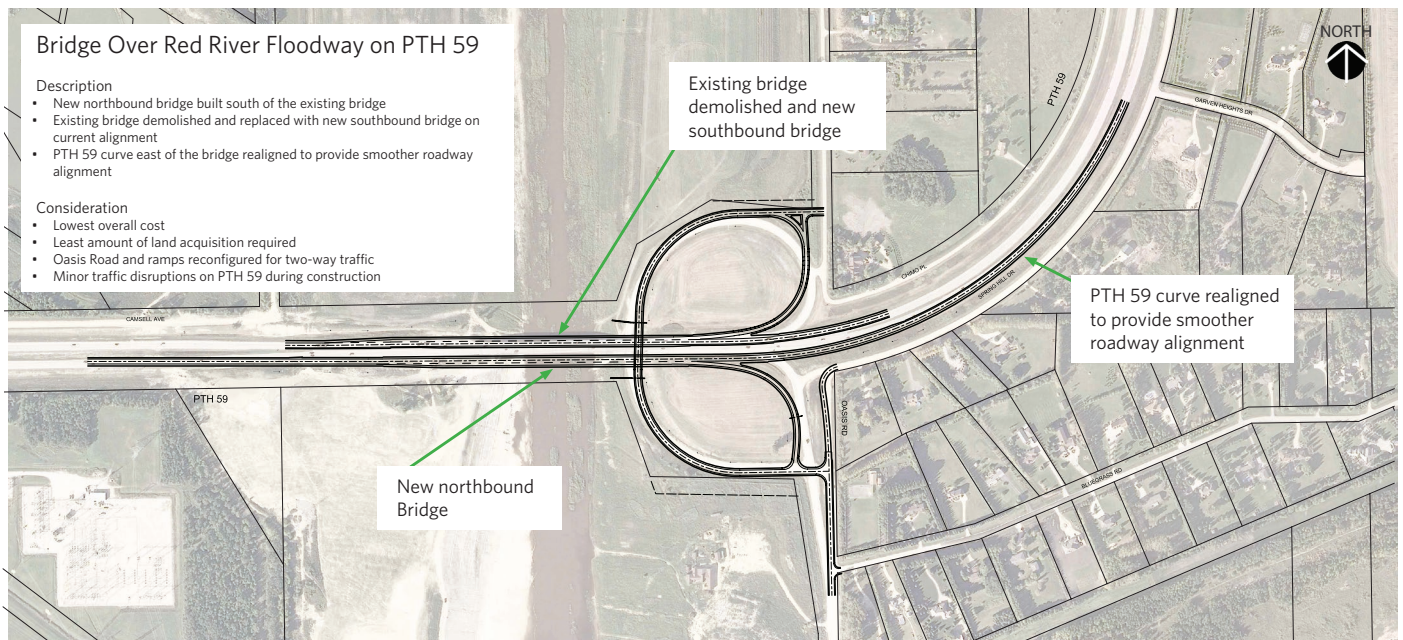
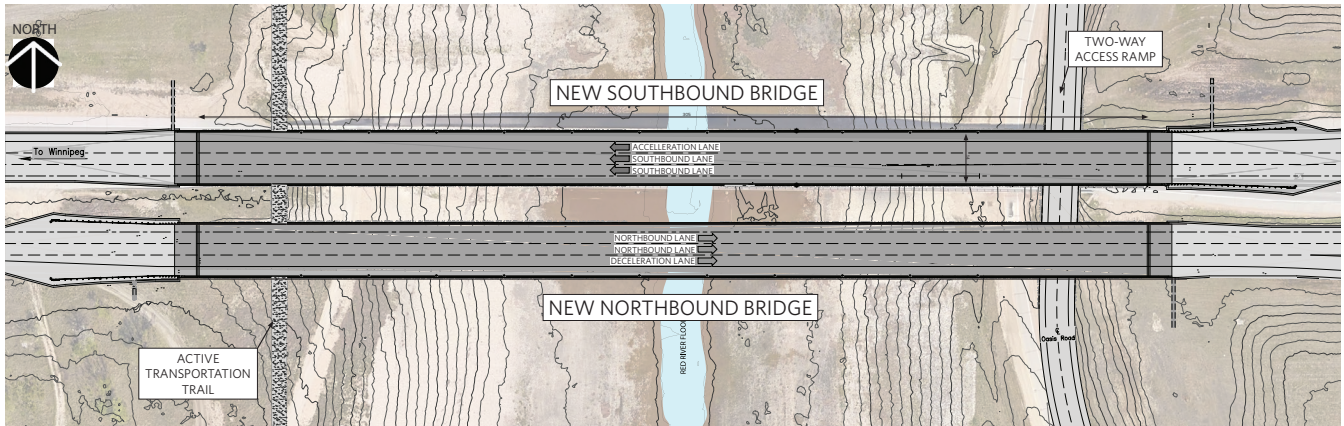


FIGURE 2 - NEW BRIDGE STRUCTURES

PLAN VIEW



CROSS SECTION

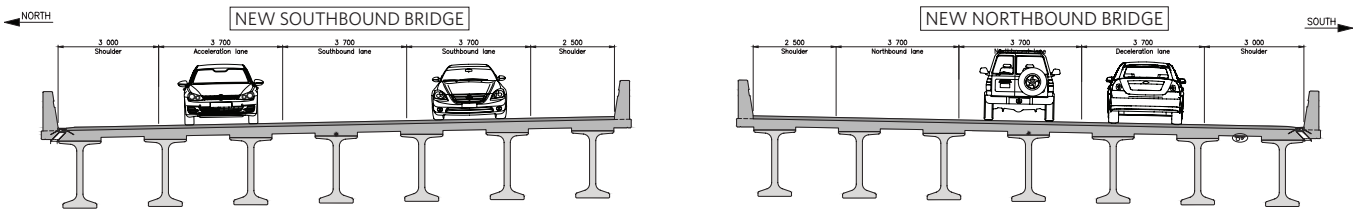


FIGURE 3 - NEW BRIDGE STRUCTURES - PLAN VIEW AND CROSS SECTION

the off-ramp from PTH 59 to Oasis Road will be periodically closed during Stage 1 (see Access Closures).

In Stage 2, traffic will move from the existing bridge to the newly constructed northbound bridge. The three lanes of the northbound bridge will be temporarily reconfigured to support four lanes of traffic: two in each of the northbound and southbound directions. The existing bridge will be demolished and the new southbound bridge will be built in its place. This crossover is estimated to occur in late Fall of 2021. During Stage 2, the off-ramp from PTH 59 to Oasis Road will be open, however the access ramp under the bridge will be closed (see Access Closures).

For the safety of the public, both the Duff Roblin Parkway and the snowmobile Trail #46 will be closed for the duration of the construction period.

ACCESS CLOSURES

While the majority of traffic on PTH 59 will be unaffected during construction, both the access ramp under the bridge and the off-ramp from PTH 59 to Oasis Road will experience periodic closures during Stage 1 and the access ramp will be closed during Stage 2.

During these closures, access between PTH 59 and Oasis Road will be via Garven Road (PR 213) and Spring Hill Drive. Increased traffic on Spring Hill Drive may cause queues at the Garven Road / PTH 59 intersection during peak periods. The geometry of the Spring Hill Drive service road and intersection movements are being reviewed

by Manitoba Infrastructure with the goals of reducing wait times and increasing safety. Updates on this review will be provided in the next newsletter issued prior to Stage 2.

ANTICIPATED CONSTRUCTION SCHEDULE

Northbound bridge and roadway construction	November 2020 to November 2021
Two-way traffic transferred to new northbound bridge (four lane traffic)	December 2021
Demolition of existing floodway bridge	January 2022
Southbound bridge and roadway construction	January 2022 to November 2022
Existing access road under bridge closed - Spring Hill Dr / Garven Road detour in effect	January 2022 to November 2022
Southbound traffic transferred to new southbound bridge	November 2022
Final paving of northbound bridge, detour removal	June 2023 to August 2023
Construction Complete	August 2023

Additional information, including previous newsletters and presentations, can be found on the project website at: www.gov.mb.ca/mit/wms/structures/design.html

If you have any questions, please contact:
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